

OVERALL SCORE

(as of April 2024)

85

✓ CR Recommended

## 2024 Subaru BRZ

CR MPG 27 mpg  
#1 of 11 Sports cars 4/5-seat

✓ **HIGHS**

- Satisfying ride/handling balance
- Short braking distances
- Quick acceleration
- Easy-to-use controls

✗ **LOWS**

- Noisy cabin
- Compromised outward views
- Difficult to get in and out of
- Minuscule rear seat

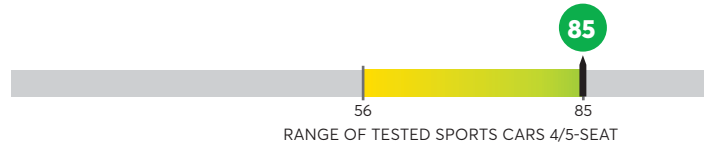
ROAD TEST



PREDICTED RELIABILITY



PREDICTED OWNER SATISFACTION



## Road Test Results

### Performance

Acceleration 0 to 60 mph	6.3 sec	5/5
Transmission		5/5
Routine Handling		5/5
Emergency Handling		5/5
Braking		5/5
Headlights		3/5

### Comfort/Convenience

Ride		3/5
Noise		2/5
Driving Position		4/5
Front Seat Comfort		4/5
Rear Seat Comfort		1/5
Front Access		2/5
Rear Access		1/5
Usability		5/5
Interior Fit & Finish		3/5
Trunk/Cargo Area		2/5
Climate System		5/5

### Fuel Economy & Emissions

Fuel Economy		4/5
CR's Overall Mileage	27 mpg	
CR's City Mileage	21 mpg	
CR's Highway Mileage	33 mpg	
Green Choice	No	
Greenhouse Gas		5/10
Smog Rating		3/10

## Survey Results



### Owner Satisfaction

2016	2017	2018	2019	2020	2021	2022	2023
NA	NA	NA	NA	NA	X	NA	!

Driving Experience		90/100
Comfort		40/100
Value		80/100
Styling		85/100

## CR Road Test Results

The formula remains unchanged for the redesigned, second-generation Subaru BRZ: This small, nimble rear-drive sports coupe is all about driving fun at a modest price. As before, the BRZ is a near-twin to the Toyota GR86, with one important difference—the BRZ's ride, while still very firm, doesn't beat you up with the constant harshness and bone-jarring character of the Toyota, which we found gets old quickly. Yet the BRZ's handling is pinpoint-precise, with a taut suspension, quick reflexes, and a forgiving nature when driven to its limits.

You'd certainly be hard-pressed to find a more alluring car to drive down a twisty two-lane road. It also proved thoroughly impressive around our track, posting a remarkable speed of 60 mph through CR's avoidance maneuver test, while managing to be a perfect dance partner for our drivers as they slung it around our twisty road course. Like the GR86, the new BRZ benefits from a larger, 2.4-liter horizontally-opposed four-cylinder engine, replacing the previous 2.0-liter version. It gained 23 horsepower in the process, now up to 228 hp. More importantly, the new engine produces considerably more power down low in the revs, with improved throttle response, both of which make it easier to drive. A six-speed automatic transmission is available, but we prefer the direct driver experience of the six-speed manual, which engages each gear with a satisfying feel. The BRZ sprinted from 0 to 60 mph in 6.3 seconds, which is competitive with other sporty models. Yet it also returned 27 mpg overall with the manual transmission, which is respectable fuel economy for a sports car.

We're happy to report that the BRZ's ride doesn't beat you

up as much as in the GR86 that we previously tested. It's still unquestionably firm, but the suspension is just absorbent enough to qualify for daily-driver duty. But, as with the GR86, a quiet cabin that insulates occupants from the outside world is not the BRZ's thing. Instead, there's an unrelenting barrage of tire rumble and engine drone that's bound to get exhausting on longer highway drives. You sit so close to the ground that you're aware of any change in the pavement texture—coarse road surfaces, for example, become immediately obvious.

Getting in and out of this small, low-slung machine can be viewed as either a chore or part of your daily exercise routine. You more "fall down into" the front seats rather than slide over and onto them, and you have to hoist yourself up to get back out. There's a tiny rear seat that only a child would want to sit in—smaller adults in a pinch (maybe) for a short drive.

The controls are simple, with an easy-to-use infotainment touchscreen plus physical buttons and knobs for the climate system that are intuitive to operate while driving. We appreciate that the tachometer is front and center within the driver's instrument cluster. Small-item storage is scarce, and the center armrest is placed too far rearward to be usable. The car's diminutive trunk dictates traveling light. Outward visibility is challenging due to short windows and thick rear pillars.

For 2024, AEB with pedestrian detection and AEB highway became standard with the manual-transmission models as well. Blind spot warning and rear cross traffic warning come standard on the top Limited trim, including the manual versions.

## Best Version to Get

Between the two trim lines, we would opt for the Limited over the base Premium—as we did with our test car—because of its stylish and grippy "Ultrasuede" seats, heated front seats, and larger 18-inch wheels with lower-profile tires, which give sharper turn-in response.

### Notable changes:

For 2024, the BRZ now comes standard with Subaru's suite of EyeSight driver-assists including models equipped with the manual transmission.

To read the full road test go to <https://www.consumerreports.org/cars/subaru/brz/2024/road-test-report/#pagestop>